

**PLANNED UNIT DEVELOPMENT
& SUBDIVISION STAFF REPORT**

Date: May 15, 2008

DEVELOPMENT NAME I-65 East Service Road Development Subdivision

SUBDIVISION NAME I-65 East Service Road Development Subdivision

LOCATION East side of East I-65 Service Road South, 1030'± North of International Drive

**CITY COUNCIL
DISTRICT** District 5

AREA OF PROPERTY 2 Lots / 32.1 ± acres (Subdivision)
2 Lots / 6.9 ± acres (PUD)

CONTEMPLATED USE Planned Unit Development Approval to allow multiple buildings on a single building site, with shared access between two building sites, and Subdivision approval to create 2 lots.

**TIME SCHEDULE
FOR DEVELOPMENT** Immediate.

ENGINEERING It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS show wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of Engineers. Must comply with all storm water and flood control ordinances. Cannot concentrate storm water runoff to an adjacent property without a release agreement or a private drainage easement. Must provide detention for any impervious area added since 1984 in excess of 4,000 square feet. Any work performed in the right of way will require a right of way permit.

TRAFFIC ENGINEERING Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Final approval for all driveways, proposed and existing will be given upon submittal of final plans.

URBAN FORESTRY

COMMENTS Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

FIRE DEPARTMENT

COMMENTS All projects must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile, and the 2003 International Existing Building Code, as appropriate.

REMARKS

The applicant is seeking Planned Unit Development Approval to allow multiple buildings on a single building site, with shared access between two building sites, and Subdivision approval to create 2 lots. The site is located in Council District 5, and according to the applicant is served by public water and sanitary sewer.

The purpose of this application is to create two lots from two parcels, and then combine one of the created lots with an existing auto dealership on an adjacent lot to create a PUD. The applicant proposed to construct a paved vehicle inventory area for the existing auto dealership.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus any changes to the site plan will require approval by the Planning Commission.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The site is zoned B-3, Community Business District, thus the auto dealer-related uses are allowed by right.

The auto inventory parking area will be accessed via a two-way entrance from the East service road for Interstate 65, as well as from the existing auto dealership to the South. The access from the service road will be controlled via gate, as the inventory parking area is not intended to be accessed by the driving public. The inventory parking includes an access route to allow emergency vehicle circulation through the inventory area, as well as tractor-trailer delivery of vehicles. Trees and a detention area are also depicted on the PUD site plan.

The existing auto dealership contains three (3) buildings, paved parking, and two curb-cuts onto the service road. With the PUD, the existing auto dealer should have adequate inventory storage on paved parking areas, thus any use of adjacent lots for inventory storage should be prohibited, unless those lots are developed in accordance with the Zoning Ordinance.

No dumpster is depicted on the PUD site plan. If a dumpster is proposed, it should be depicted on the site plan, or a statement should be provided indicating that no dumpster will be included as part of the development. Furthermore, the development of the inventory parking area must comply with all other codes and ordinances, including the lighting requirements of the Zoning Ordinance.

The site will have to comply with the City of Mobile storm water and flood control ordinances. The storm water detention system and best management practices for erosion and sediment control should be in place early in the site development process.

Regarding the proposed two-lot Subdivision, proposed Lot 1 has frontage on both the Interstate 65 Service Road as well as a private street associated with Colonial Bel Air Mall. Lot 1 may also have a small amount of frontage onto International Drive. Proposed Lot 2 only has frontage onto the Interstate 65 Service Road. Lot 1 should be limited to two (2) curb-cuts onto the service road (*and one (1) curb-cut to International Drive, if adequate frontage exists*), while Lot 2 should be limited to one curb-cut, with the size, design and location to be approved by Traffic Engineering and ALDOT as necessary, and in conformance with AASHTO standards. Curb-cuts for Lot 1 to the private street associated with Colonial Bel Air Mall should have their size, design and location approved by Traffic Engineering, and be designed in conformance with AASHTO standards.

The 25-foot minimum building setback line is not depicted on the Subdivision plat or the PUD site plan, but is required. Both drawings should be revised to depict the setback line from all public and private streets.

The geographic area defined by the city of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

Much of the Subdivision site appears to contain wetlands, as identified by available National Wetlands Inventory data, and some wetland areas are delineated on the PUD site plan, but not on the Subdivision plat: wetlands should be delineated on both the site plan and the plat. The potential presence of wetlands indicate that the area may be environmentally sensitive; therefore, the approval of all applicable federal, state and local agencies would be required prior to the issuance of any permits or land disturbance activities.

RECOMMENDATION

Subdivision: Based upon the preceding, this application is recommended for Tentative Approval, subject to the following conditions:

- 1) Compliance with Engineering comments (*It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS show wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of Engineers. Must comply with all storm water and flood control ordinances. Cannot concentrate storm water runoff to an adjacent property without a release agreement or a private drainage easement. Must provide detention for any impervious area added since 1984 in excess of 4,000 square feet. Any work performed in the right of way will require a right of way permit*);
- 2) Placement of a note on the final plat stating that Lot 1 is limited to two (2) curb-cuts onto the Interstate 65 Service Road (and one (1) curb-cut onto International Drive if at least 50-feet of frontage exists), that Lot 2 is limited to one (1) curb-cut onto the Interstate 65

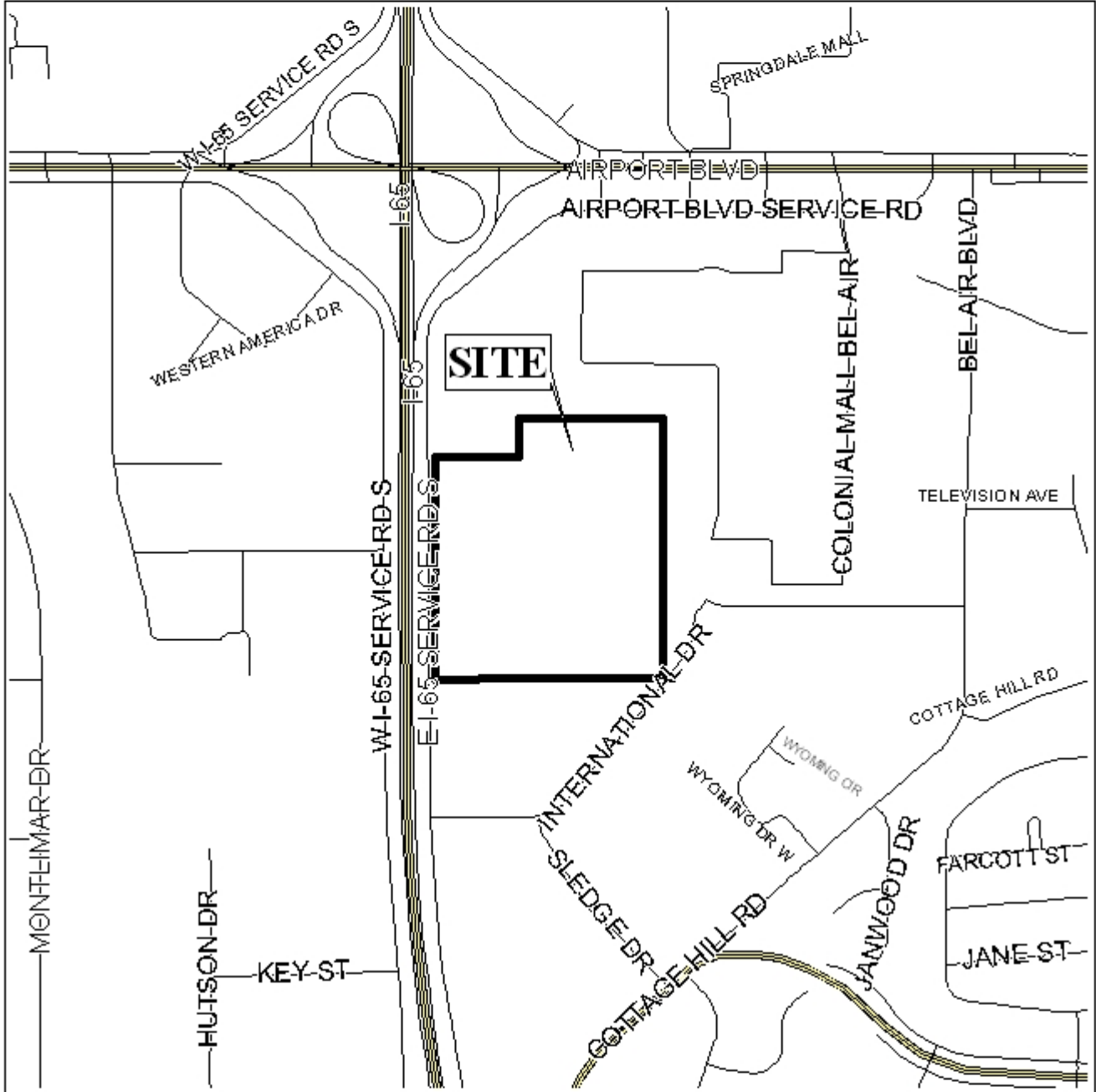
- Service Road, with the size, design and location to be approved by Traffic Engineering and ALDOT as necessary, and in conformance with AASHTO standards;
- 3) Placement of a note on the final plat stating that the size, design and location of curb-cuts for Lot 1 onto the private street associated with Colonial Bel Air Mall must be approved by Traffic Engineering, and be designed in conformance with AASHTO standards;
 - 4) Use of “best management practices” during site development, in compliance with Section V.A.5. of the Subdivision Regulations, to minimize erosion and sedimentation during site development;
 - 5) Delineation of wetlands on the final plat, if any, and the approval of all applicable federal, state and local agencies regarding wetlands prior to the issuance of any permits or land disturbance activities;
 - 6) Placement of a note on the final plat stating that development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species;
 - 7) Revision of the Subdivision plat and PUD site plan to depict a 25-foot minimum building setback line from all public and private streets;
 - 8) Provision of a revised PUD site plan;
 - 9) Completion of the Subdivision process prior to the request for any permits, including for land disturbance; and
 - 10) Full compliance with all other municipal codes and ordinances.

Planned Unit Development: Based upon the preceding, this application is recommended for Approval, subject to the following conditions:

- 1) Compliance with Engineering comments (*It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS show wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of Engineers. Must comply with all storm water and flood control ordinances. Cannot concentrate storm water runoff to an adjacent property without a release agreement or a private drainage easement. Must provide detention for any impervious area added since 1984 in excess of 4,000 square feet. Any work performed in the right of way will require a right of way permit*);
- 2) Design of all curb-cuts to be approved by Traffic Engineering and comply with AASHTO standards;
- 3) Delineation of wetlands on the site plan, if any, and the approval of all applicable federal, state and local agencies regarding wetlands prior to the issuance of any permits or land disturbance activities;
- 4) Placement of a note on the site plan stating that development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species;
- 5) Revision of the Subdivision plat and PUD site plan to depict a 25-foot minimum building setback line from all public and private streets;
- 6) Revision of the site plan to include a statement regarding the provision of a dumpster, or placement of a note on the site plan stating that dumpsters will not be provided as part of the development;

- 7) Placement of a note on the PUD site plan stating that cross-access easement is limited to between the proposed Lot 2, I-65 East Service Road Development Subdivision, and existing Lot 1, Interstate SE Subdivision, and that no vehicle inventory will be stored on adjacent lots until those lots are brought into compliance with the Zoning Ordinance;
- 8) Provision of a revised PUD site plan prior to the signing of the final plat;
- 9) Completion of the Subdivision process prior to the request for any permits, including for land disturbance; and
- 10) Full compliance with all other municipal codes and ordinances.

LOCATOR MAP



APPLICATION NUMBER 26 & 27 DATE May 15, 2008

APPLICANT I-65 East Service Road Development Subdivision

REQUEST Subdivision, Planned Unit Development



NTS

PLANNING COMMISSION VICINITY MAP - EXISTING ZONING

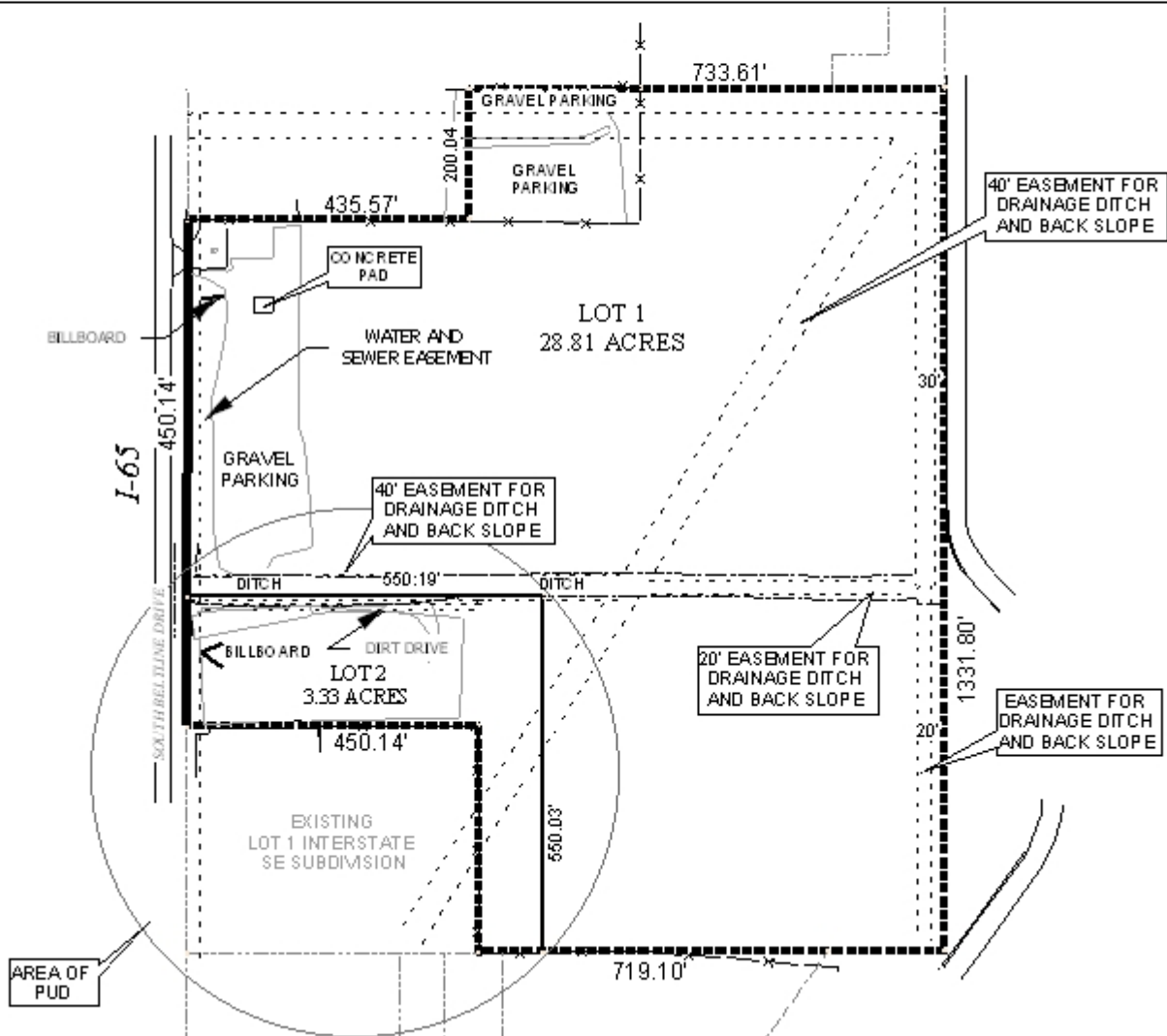


The site is surrounded by commercial land use; auto sales to the north and south, retail to the east, and hotels to the west.

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LEGEND R-1 R-2 R-3 R-A R-B H-B T-B B-1 LB-2 B-2 B-3 B-4 B-5 I-1 I-2 NTS

SUBDIVISION DETAIL



The easements, existing parking, existing ditch, and billboards are illustrated in the site plan.

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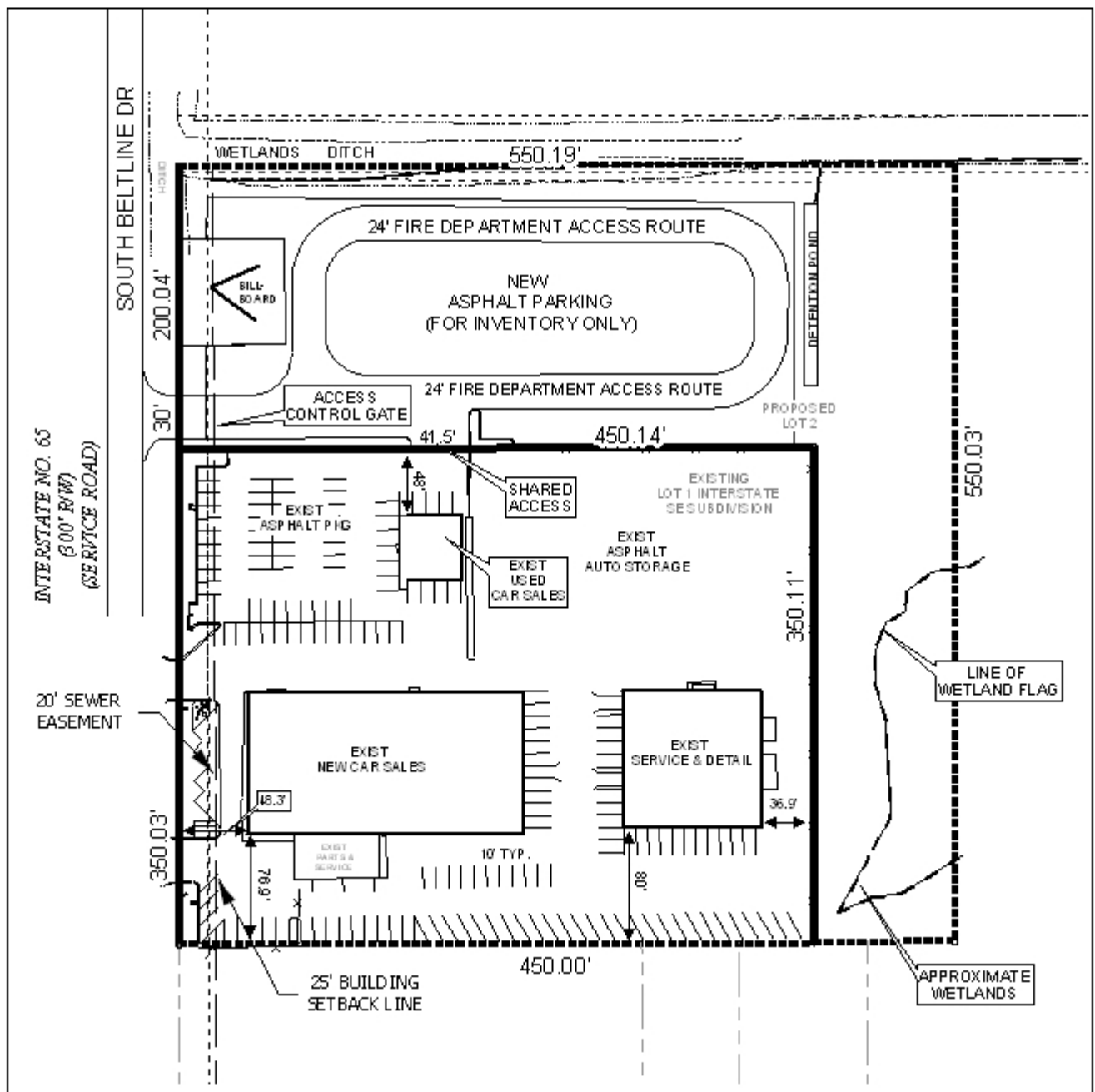
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NTS

PLANNED UNIT DEVELOPMENT DETAIL



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